

Top right: Looking north on Heister Street towards College Avenue

Middle right: Outdoor dining/seating on west side of Heister Street.

Bottom right: Example of a "pop-up cafe." As on Allen Street, these temporary seating areas would add activity and color to the street as well as offer dining spaces that do not currently exist downtown.

3-J: Heister Promenade

Implementation: *Borough of State College, Design Review Board, PSU Landscape Architect, Consultants*

Streetscape Type A - Primary

Concept: Consider allowing the 100 block of Heister Street to function similar to the 100 block of South Allen where it is closed on occasion or during special event weekends. This could be particularly valuable on the east end of downtown to provide larger gathering areas and relief from crowding along the Beaver Avenue sidewalks. This block of Heister and the connecting block of Calder Way currently have restaurants and outdoor dining areas that activate the edges, reinforcing the desirability of this street over others for temporary closures. Any redevelopment considered for the surface parking lot adjacent to this block of Heister Street should consider how ground floor uses could further activate the Heister Street frontage. Refer to Theme Four recommendations for a description of development/redevelopment potential.

While the Heister Street Promenade will function similarly to the Allen Street Promenade, the design treatment does not need to be taken to the same level, as temporary closures for programming would likely be fewer than for Allen Street. The street would maintain curbs and would not include extensive special paving. If temporary closures of the street and programming are successful, then long-term consideration might be given to doing a more elaborate design treatment, similar to the Allen Street Promenade.

Other streets identified by stakeholders as alternatives to Heister street for temporary closures include Garner Street and Locust Lane. Garner Street, while a good option for temporary closure in terms of uses and location, is an important connecting street to the University and areas to the south. Therefore, periodic closures would be likely be problematic. Some stakeholders suggested Locust Lane because it is currently closed regularly for the farmers market. However, it lacks the appropriate uses along the edges necessary to activate the space.



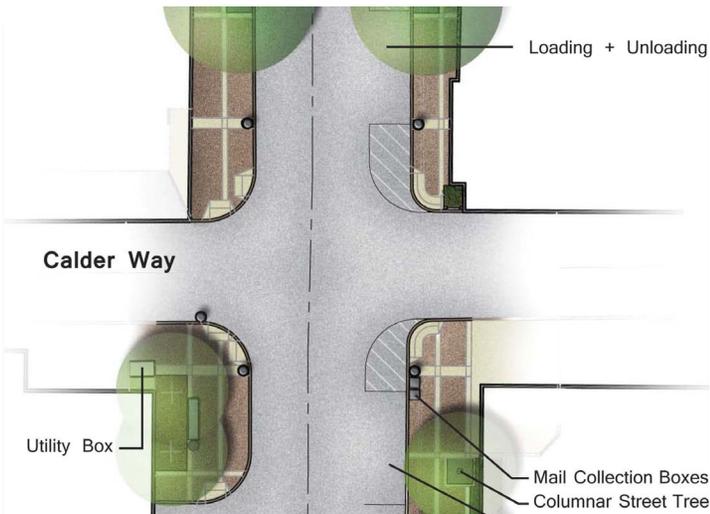
**Recommendation 3-K
Pugh Street
(Catalyst Project - All Sections Except Pugh Street
Garage Frontage)**

Proceed with the current streetscape design with minor modifications to the paving pattern and implement as part of the Catalyst project.

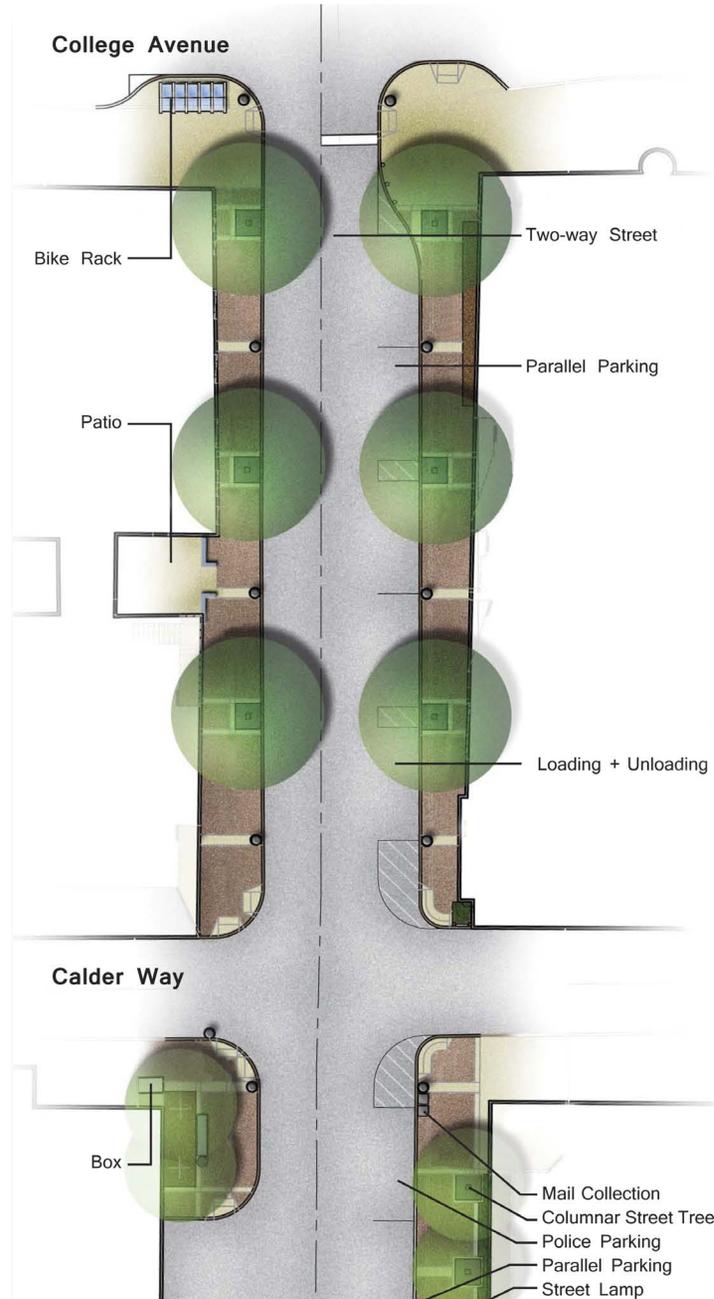
Implementation: Borough of State College

Streetscape Type B - Primary

Concept: Pugh Street functions as the “bookend” to the heart of the downtown core area, with Fraser Street functioning as the other bookend. The current design concept that matches Fraser Street and provides expanded sidewalk areas in some locations is a good one. The materials and design should reflect the Fraser Street streetscape; however, consideration should be given to eliminating the center concrete band that runs the length of the sidewalk area to simplify the pavement pattern. Because the Pugh Street garage will be replaced in the near future, the streetscape improvements might focus on the stretch between College Avenue and Calder Way for both sides and only the east side between Calder Way and Beaver Avenue.



Current Concept (By Others)



Recommendations: Eliminate Linear Concrete Band

Left: The current streetscape design concept for Pugh Street includes concrete banding along the length of the sidewalk.

Far left: It is recommended that the streetscape design concept be slightly modified to eliminate the concrete banding that runs parallel to Pugh Street. The banding running perpendicular to Pugh Street should remain as shown.

3-L: College Avenue (West of Atherton Street)

Provide pedestrian and bicycle enhancements with road diets as part of PennDOT Turnback Program.

Implementation: *Borough of State College, Penn State University, Neighboring Townships*

Streetscape Type C

As described earlier, College Avenue between Atherton and Buckhout Streets is being considered for road diets to reduce to one travel lane. With these lane reductions, consider the addition of parallel parking on one side of the street and a 5' bike lane on the opposite side. As redevelopment occurs along West College Avenue as part of the West Side Revitalization, the parallel parking could support small-scale commercial uses as well as promote a traffic calming effect.

The concept of reducing College Avenue to one lane has sparked much debate within the community. Prior to any detail design, further traffic studies should be conducted as well as providing additional opportunities for community input. Additionally, considerations should be given to "testing" the improvements with temporary pavement markings before making permanent changes.

Design materials for these streets will utilize the downtown standards. Paving will be predominantly concrete with brick accents in key areas, however, brick should be emphasized in the vicinity of Sparks Street and the proposed West Side Square as described below. As detail designs are developed, the design should be coordinated with the Ferguson Township streetscape improvements currently underway for areas west of Buckhout Street to create an appropriate transition.

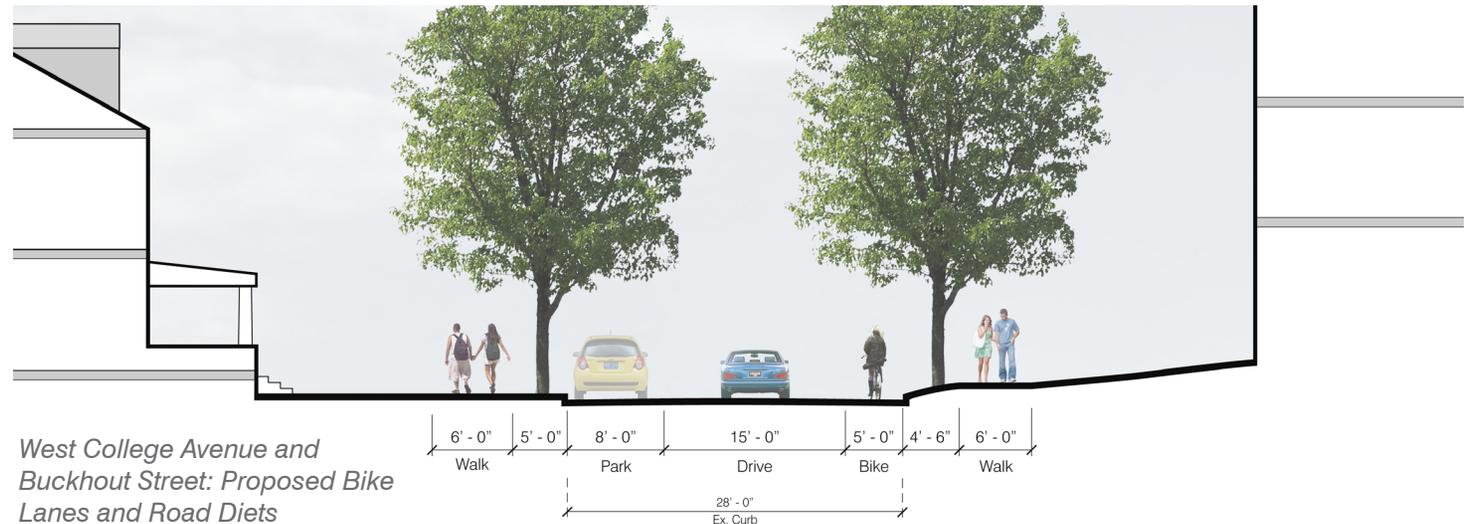
Coordination: It will be important that the detailed planning and design for College Avenue is a coordinated planning effort among the Borough, University, and College and Ferguson Townships, particularly at gateways

**Recommendation 3-M
West Campus Drive and Other West End Streets**

Reinforce the town-gown identity between the West End Urban Village and Penn State's West Campus.

Implementation: *Borough of State College, Penn State University*

Streetscape Type A - Secondary



West Campus Drive should be designed similar to College Avenue (west of Garner Street), utilizing the same design standards and creating a pedestrian-friendly urban streetscape. This treatment should extend around the street network that ultimately defines the new West End Commons which will likely include Sparks Street and a segment of West College Avenue.

Recommendation 3-N

Other Streets

Incrementally improve other downtown streets to complete the streetscape network.

*Implementation: **Borough of State College***

Streetscape Type E

Concept: Less significant streets will utilize the same family of materials but not to the same level of intensity as streets described above, particularly as it relates to the use of special paving. The intent is that these streets are enhanced and convey the downtown image but are clearly lower in the hierarchy than College, Allen, Beaver, Fraser, Pugh and Garner Streets, among others.

Specific design criteria is described in Appendix C: Design Guide.

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