



Beaver Avenue

Focus Project 4 (Catalyst - Miller Alley to Pugh Street)

Recommendation 3-I Beaver Avenue Streetscape Enhancements

Modify Beaver Avenue to enhance aesthetics and expand pedestrian areas where possible. Implement as part of Catalyst project.

Implementation: **Borough of State College, Design Review Board**

Streetscape Type B - Primary and Secondary

Concept: Narrow travel lanes on Beaver Avenue and along High Street to College Avenue to provide wider sidewalks and gathering areas. Between Garner and Atherton Streets, maximize opportunities to widen sidewalks and provide extended sidewalk bulb-outs such as those improvements implemented between Fraser and Allen Streets. Between Garner and High Street, widen sidewalk areas in conjunction with narrowed travel lanes. Specific design considerations include:

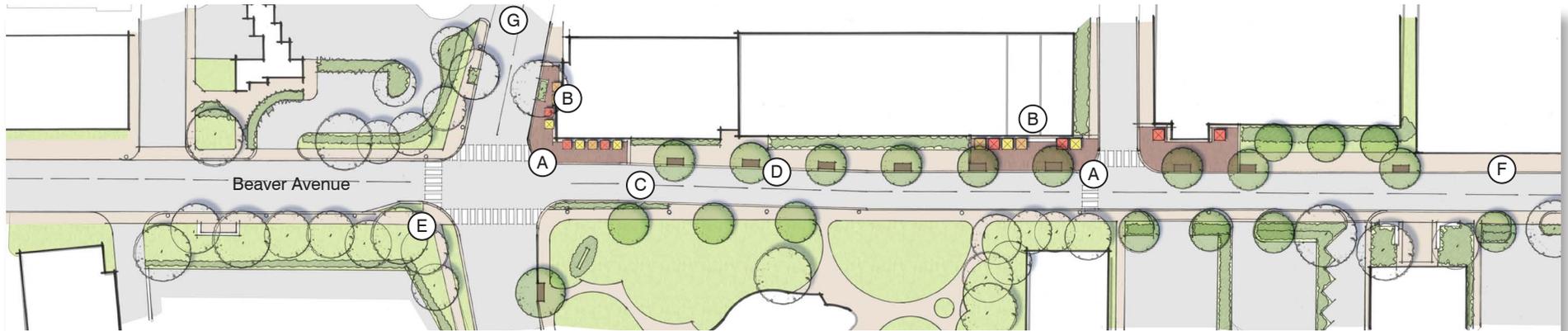
Branding and Identity: Develop a unique identity for Beaver Avenue. Beaver Avenue is a unique street that transforms in character from east to west. There is an opportunity to brand Beaver Avenue and capitalize on this with the tag line “All kinds of character.” This provides the opportunity to play up the traditional downtown character west of Pugh Street and a more student-oriented identity east of Pugh Street. Creating a separate brand identity for Beaver Avenue is more of a long term recommendation. The opportunity is to develop an identity that incorporates a bolder “collegiate” block letter motif along with banners. Initially, however, Beaver Avenue should incorporate the overall downtown brand.

Lane Narrowing and Sidewalk Expansion: Where possible, particularly between Garner and High Streets, narrow lanes from 15’ wide each to 12’ wide. This will allow for sidewalk expansion of approximately 3’ on each side.

Top right: The brand identity and tagline for Beaver Avenue emphasize that Beaver Avenue is a unique street that transforms in character from east to west, from student-oriented to traditional downtown, respectively.

Bottom right: Banners help to reinforce Beaver Avenue’s unique identity inside of the larger downtown.





Beaver Ave. Concept Drawings:
Illustrative Plan - Typical Segment

- A. Large Fields of Brick Pavement at Intersections
- B. Opportunity for Outdoor Seating/Dining in Expanded Sidewalk
- C. Lane Shift to Allow for expanded sidewalks
- D. New Street Trees in Tree Grates
- E. Bulb-Outs
- F. Expanded Sidewalk
- G. Sharrows to Designate Garner Street as Bike Route Until Bike Lanes Can Be Added (If Feasible)

Pavement: Between Fraser and Pugh Streets, utilize the paving pattern already utilized on some sections of the Beaver Avenue sidewalk (brick with concrete banding). Beyond Fraser and Pugh Streets, in each direction, utilize primarily scored concrete with large brick fields at intersections.

Transit Stops: As described earlier under Theme 2, enhance the transit stops along Beaver Avenue. Specifically, consider relocating the existing stop on the west side of Garner Street to the east side if it cannot be enhanced in its existing location. Also, work with the Church of Jesus Christ of the Latter Day Saints (LDS) to explore the potential of an easement on their property to provide more gathering space and access to the existing transit stop near High Street. This will require a modification to their approved site plan for the Gospel and Worship Center.

Beaver Ave. Concept Drawings:
Cross Section Between Garner Street and Hetzel Street

